



SPOA BUSINESS MANAGEMENT

DELIVERY & COLLECTION OF PLANT (2)

GLASGOW TRAINING GROUP LIMITED

Code of Practice

Lighting and Marking for Abnormal Load Self escorting
vehicles incorporating Operating guidance



HIGHWAYS
AGENCY

Safe roads, reliable journeys, informed travellers

Code of Practice

Lighting and Marking for Abnormal Load Self escorting vehicles incorporating Operating guidance

TABLE OF CONTENTS

1	INTRODUCTION	4
2	VEHICLE REQUIREMENTS	5
	Size and type.....	5
	Colour.....	5
	Lighting.....	5
	Markings and signing – see figures 1 & 2	6
	Side markings.....	7
	Rear markings	7
	Equipment and information to be carried in escort vehicle	11
	Road side safety equipment.....	11
	Emergency safety equipment.....	11
	Job specific reference information.....	11
	<i>Communications equipment</i>	11
3	ESCORT PERSON SPECIFICATION	12
	Escort person	12
	Main duties and responsibilities	12
	Essential skills and qualifications	13
	Desirable skills and qualifications.....	13
	Personal protective equipment.....	13
	Self certification	14
4	OPERATION	14
	Escorting criteria.....	14
	Operating guidance	14
5	BEFORE STARTING THE JOURNEY	15
	Escort vehicle check.....	15
	Pre-journey checks with abnormal load or abnormal vehicle personnel.....	15
6	ON THE ROAD.....	16
	Basic principles on vehicle manoeuvres	17
	Manoeuvre to close a lane to the left	17
	Manoeuvre to close a lane to the right	17
	Procedure for entering a motorway (principally for slow wide loads).....	17
	Procedure for leaving a motorway.....	18

Basic principles – escorting from the front	19
In the event of a breakdown of the abnormal load vehicle.....	19
7 PARKING UP	20
Vehicle parked “In line”	20
Vehicle parked in “Fend off” position.....	21
Vehicle parked in “Fend in” position.....	21
8 JOB COMPLETED.....	22
APPENDIX 1: REFERENCES	23
APPENDIX 2: SELF ESCORTING DECLARATION	24
APPENDIX 3: ABNORMAL LOAD CLASSIFICATIONS AND NOTIFICATIONS.....	25
APPENDIX 4: ORGANISATIONAL ENDORSEMENT	26

1 Introduction

- 1.1 This code of practice is voluntary and does not prescribe hard and fast rules, but provides practical guidelines, which may be departed from in appropriate circumstances. It is not intended to be a comprehensive statement of the law's requirements.
- 1.2 This code relates to the escorting of both abnormal loads and abnormal vehicles; throughout the document where reference is made to abnormal load it also refers to abnormal vehicle.
- 1.3 This said it is an authoritative statement of best practice, which has been agreed to by the organisations listed in Appendix 4. It not only points out many of the legal obligations which need to be met, but also contains information and explanations which are designed to ensure that those escorting abnormal loads¹ on the highways do so in a manner which maximises safety.
- 1.4 Self escorting offers greater flexibility for scheduling moves and therefore should enable loads to travel at off peak times when there is less traffic. This both reduces traffic disruption to other road users and improves journey times for the haulier.
- 1.5 Legislation² requires hauliers to notify the movement of most abnormal loads to the police before moving them by road. There is also a requirement to notify heavier loads and vehicles to highway and bridge authorities to ensure that bridge structures can accommodate these heavy loads. In Scotland, the term highway authority shall be deemed to be road authority. For the largest and heaviest abnormal loads, prior permission before moving is required from the Highways Agency Abnormal Loads team. If it is an abnormal vehicle, authorisation is required from the Vehicle Certification Agency.
- 1.6 Hauliers involved in the movement of abnormal loads have a responsibility to ensure that each movement is conducted in a controlled and safe manner. This shall equally apply if the load is being self escorted. Some larger and heavier loads are required to have an attendant, as defined in the relevant legislation³.
- 1.7 The term self escorting applies to either a haulier or a private company engaged by a haulier escorting an abnormal load. An escort person has no powers to stop or direct other road users or pedestrians and is there to warn them of the presence of an abnormal load.
- 1.8 This code of practice, which applies throughout England, Wales and Scotland, is intended to provide a nationwide standard that is recognised by both industry and the police authorities alike. A standard livery helps the general public and in particular

¹ Abnormal Indivisible Load as defined in Schedule 1, paragraph 2 by The Road Vehicles (Authorisation of Special Types) (General) Order 2003 (SI 1998) and Indivisible Load as defined by Regulation 3(2) of The Road Vehicles (Construction & Use) Regulations 1986 (SI 1078)-namely a load that cannot without undue expense or risk or damage be divided into two or more loads for the purpose of being carried on a road. STGO 2003 limits gross weight to 150 tonnes, length to 30m and width to 6.1m, above which a Special Order is required from the Highways Agency. A Highways Agency VR1 permit is required if width exceeds 5m.

² Schedule 12 of The Road Vehicles (Construction & Use Regulations) 1986 as amended (SI 1986/1078). Part 2 of The Road Vehicles (Authorisation of Special Types) (General) Order 2003 (SI 1998).

³ The Road Vehicles (Construction & Use Regulations) 1986 (SI 1078), Schedule 12 paragraph 2. Part 2 of The Road Vehicles (Authorisation of Special Types) (General) Order 2003 (SI 1998)

other road users to recognise these vehicles, thereby alerting them to the movement of an abnormal load so they can exercise greater caution as appropriate. However, to reduce the burden on industry, application of the code of practice is for new vehicles only.

1.9 This code of practice covers:

- the requirements for an escort vehicle including details of its appearance, markings, warning beacons, and equipment to be carried⁴
- the person specification for a self escort person with a description of the roles, responsibilities and duties
- general operating practices, including thresholds for self escorting, documentation to be carried and practices to be adopted whilst actively engaged in self escorting.

1.10 There is an additional code of practice which offer recommendations on the lighting and marking of abnormal load vehicles.

- Lighting and marking code of practice for SO, VR1, STGO and C&U loads.

2 Vehicle requirements

Vehicle specification

Size and type

- 2.1 An escort motor vehicle shall have a minimum of four wheels.
- 2.2 The escort vehicle must afford the driver good visibility. To facilitate this to the rear and side, the vehicle shall be fitted with exterior mirrors on both sides. In addition, it is preferable to have rear windows at the back and to the sides to aid driver visibility. Vehicles with open cargo space are not permitted.
- 2.3 The escort vehicle shall not form any part of the units carrying or hauling the abnormal load.

Colour

- 2.4 To be conspicuous to other road users and pedestrians the escort vehicle shall be of a uniform colour; white, yellow or orange is recommended. Silver is not considered to be a conspicuous colour; however it may be used together with appropriate retroreflective markings that make the vehicle conspicuous.

Lighting

- 2.5 In addition to the obligatory vehicle lighting requirements as specified in the Road Vehicles Lighting Regulations (RVLR) (1989) flashing warning beacons⁵ shall be fitted

⁴ References to equipment standards throughout the code of practice also cover Turkish standards and standards of EFTA states that are contracting parties to the EEA Agreement.

⁵ Beacon defined as visible around 360°

to the roof of the escort vehicle to alert other road users and pedestrians to the presence of an abnormal load or abnormal vehicle. This shall be a full roof width light bar fitted transversely with amber lights at each end and visible from the front and rear of the vehicle. No more than two banks of lights are permitted. These may be demountable. Warning beacons shall conform to both RVLR requirements and Economic Commission for Europe (ECE) Regulation 65 on Special Warning Lamps.

- 2.6 The amber warning beacons shall only be used whilst the vehicle is actively engaged in escorting an abnormal load.
- 2.7 Flash rates over 10Hz have been shown to increase glare and can also increase the risk of epileptic response. Alternating beacons (right and then left) at 1-3Hz increases the conspicuity of the vehicle without an increase in glare or increasing the risk of an epileptic response.
- 2.8 Beam cut-off has an effect on glare; it is recommended that the intensity at night with beam cut-off is 325-425cd and without beam cut-off it is 150-200cd. To minimise the safety risk from glare, beacons shall not be placed at the eye level of drivers.
- 2.9 To help alert other road users approaching the escort vehicle from the front, during the day the vehicle shall use continuously illuminated dipped beam headlamps when escorting the abnormal load.

Markings and signing – see figures 1 & 2

- 2.10 The vehicle shall be marked clearly to identify that it is an escort vehicle. This shall be done by marking "Escort Vehicle" or "Escort" in solid black first letter upper case, remaining lower case lettering, or lower case in forward script on the front of the vehicle. The marking shall be in plain sans serif font (e.g. Helvetica or Arial), and the lettering a minimum of 150mm high and a width to height ratio approaching one. Shadowing, italics and other lettering effects are not permitted. No other text or logos shall be located near to this lettering.
- 2.11 In addition, a fixed or removable sign may be fitted to the front and rear of the vehicle. The sign should only be displayed when the vehicle is actively engaged in escorting an abnormal load. The wording on this sign shall be ABNORMAL LOAD or WIDE LOAD as standard with SLOW LORRY or LONG LOAD as an option. The sign shall be mounted such that the sign face is perpendicular to the road surface and the text is horizontal. The sign shall be mounted either on the roof of the escort vehicle or preferably below the front grille, ensuring that the front number plate is still legible and does not affect the operation of the vehicle. The rear sign shall be mounted preferably below the rear number plate of the vehicle. If the sign is roof mounted, it must not obscure the roof mounted flashing amber warning beacon.
- 2.12 The sign should have black lettering on a yellow retroreflective background. The height of the lettering shall be a minimum height of 100mm. If the sign is mounted below the front grille of the escort vehicle, the retroreflective material used shall be to a minimum standard of Class RA1 or equivalent. The rear sign shall display the same legend as the front mounted sign and conform to the same standard of material. If the sign is mounted on the roof of the escort vehicle, the retroreflective material used shall be to a minimum standard of Class RA2 or equivalent.

- 2.13 Marker boards may be fitted to both ends of the sign. The width of the marker board and associated plates shall be the same as the width of the vehicle.



- 2.14 There shall be a roof sign displaying "Abnormal Load" in first letter upper case, remaining lower case lettering or lower case lettering using a sans serif font (e.g. Helvetica or Arial) and with a width to height ratio approaching one, which should be clearly visible from both the front and rear. This sign shall be lit when escorting and not be illuminated at any other time. Lighting regulations only permit red lighting to face the rear of the vehicle and any colour other than red to face forward. This should be taken into consideration in the design of any sign (suggested red internal lighting to rear and white to front). This sign may be demountable.

Side markings

- 2.15 To alert other road users approaching the vehicle from the side, a continuous 200mm wide horizontal strip of yellow retroreflective material shall be fitted along the length of the vehicle to at least the front wheel arch. At the rear of the vehicle a 300mm wide vertical chevron strip of retroreflective yellow and non retroreflective red shall be fitted. An alternative option shall be to block fill the side of the vehicle with fluorescent material.
- 2.16 The name of the vehicle operator and/or owner may be displayed upon the side of the vehicle forward of the extended chevron markings. The name and any logos shall not be retroreflective. No company livery shall be displayed on the front or rear of the vehicle. In addition, the following rules shall be followed:
- the number of the letters/characters shall be less than fifteen
 - the height of the letters/characters is between 300mm and 1,000mm
 - no long designs are used eg addresses and phone numbers.

Rear markings

- 2.17 To help alert other road users approaching the escort vehicle from the rear, high visibility markings shall be applied to the rear of the vehicle. These will be chevron markings of alternate red retroreflective material and yellow vinyl non retroreflective material (or equivalent), each with a strip width of 250mm on vans and larger vehicles, and 150mm for cars and car-derived vans.
- 2.18 The chevron markings shall be at 60° to the horizontal plane, with the chevrons pointing upwards and matching colours meeting at the vertical centre line of the vehicle. As much of the rear of the vehicle as possible should be marked in this way and any vehicle fittings should be cut around, eg light assemblies, number plates etc.
- 2.19 In addition to the chevron markings on the rear of the vehicle, outline markings consisting of 25mm-50mm wide red microprismatic retroreflective material shall be

fitted along the roof edge and to the outside door pillars in the case of the van and outside edge of the rear window in the case of a car.

- 2.20 When the rear doors of a self escort van are opened a 25mm–50mm wide red microprismatic retroreflective strip of material shall be fitted along the rear facing edge of the doors, along the open roof edge and along the floor sill. In the case of a self escort car the strip of material shall be fitted to the rear facing edge of the boot lid or tailgate.
- 2.21 If a piece of equipment is fitted or attached to the rear of the escort vehicle and obscures the chevron markings (eg a crash cushion in the upright position), the chevron markings shall be replicated on the equipment, so that the pattern continues to be visible.
- 2.22 To maintain high vehicle visibility from different angles, the red and yellow chevron markings shall be continued in an unbroken pattern onto the side of the vehicle body for a distance of 300mm measured from the rear most corner of the vehicle body. The markings shall be over the full height of the vehicle body.

Figure 1 – Illustration of an escort vehicle (not to scale)

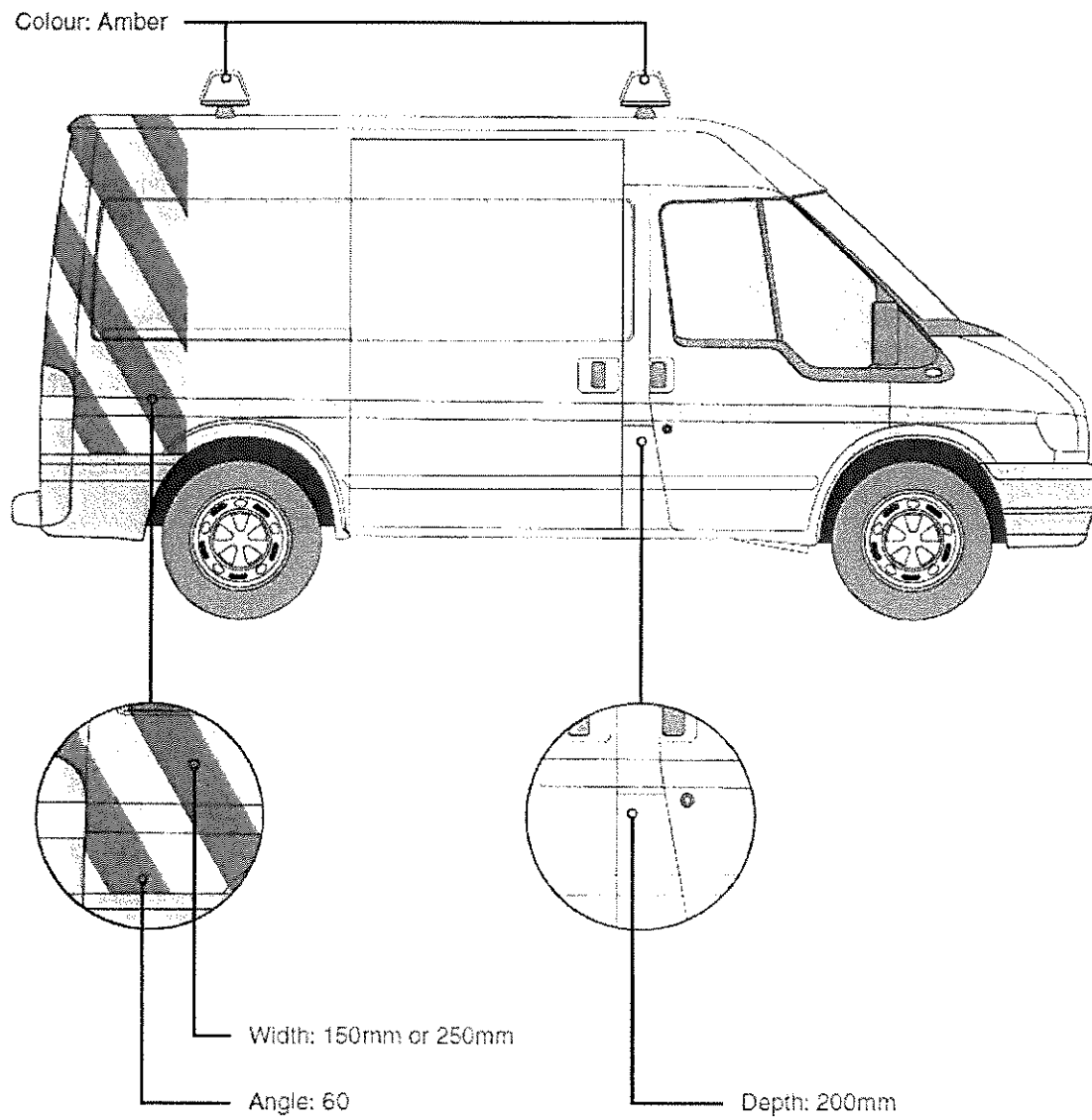
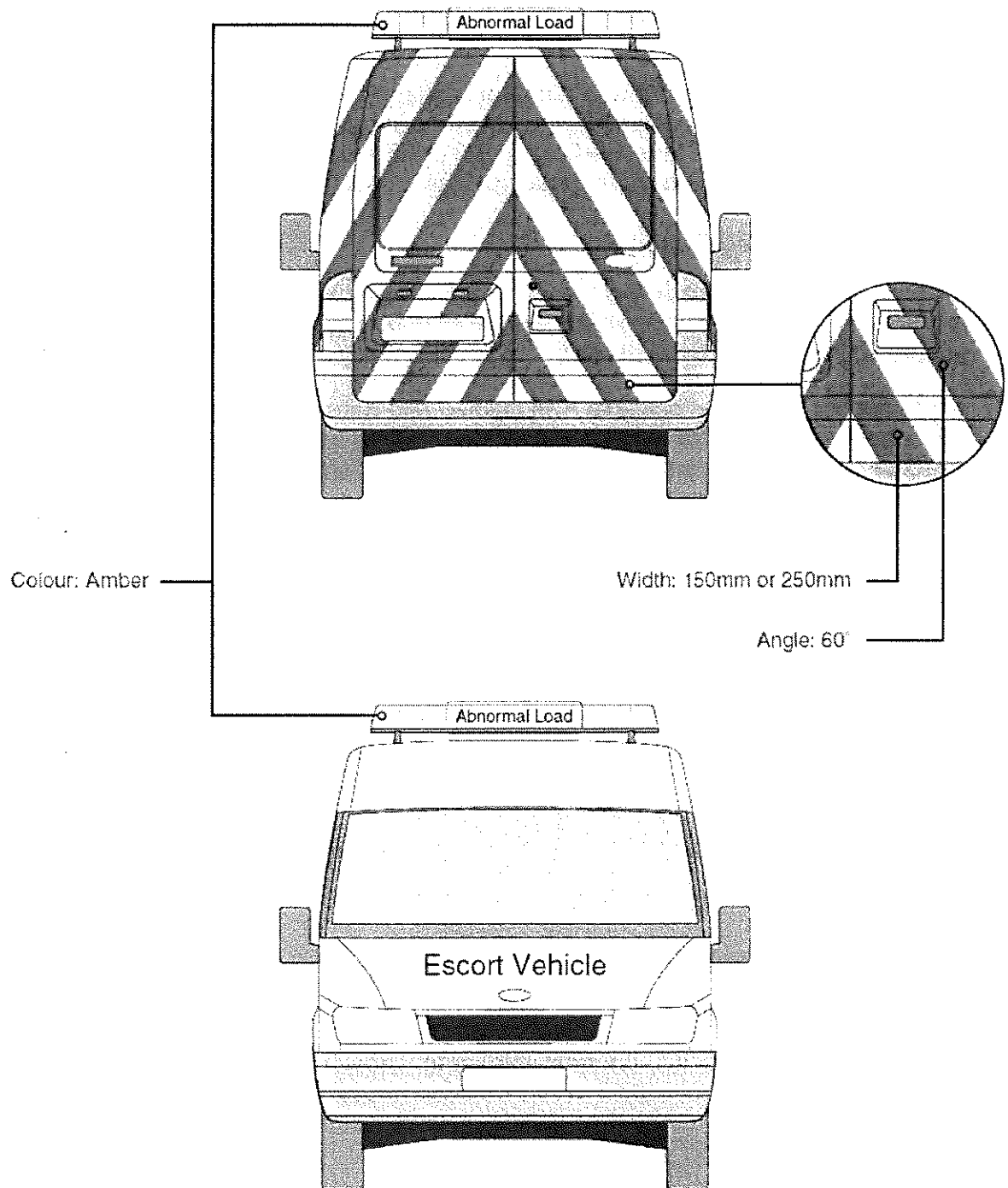


Figure 2 – Illustration of an escort vehicle (not to scale)



Equipment and information to be carried in escort vehicle

Road side safety equipment

2.23 To effectively manage road side safety during planned or unplanned stops, each escort vehicle shall be equipped with the following:

- A minimum of 12 traffic cones to BS 873:Part 8:1985⁶, and diagram 7101.1 in TSRGD 2002⁷, or an EC or EEA standard, which in practice provides standards equivalent to those standards contained in a British Standard.
- A minimum of 4 lighting units with a rate of flashing for each unit between 60 and 240 flashes per minute, compatible for installation upon traffic cones described above, to BS 3143:Part 2:1990 as amended⁸ or an EC or EEA standard, which in practice provides standards equivalent to those standards contained in a British Standard.

Emergency safety equipment

2.24 In the event of an incident during the journey, it is expected that the escort vehicle will be first to the scene. Therefore each vehicle shall be equipped with as a minimum, (and personnel trained in the use of):

- a powder or CO₂ fire extinguisher Kite marked and British Approval for Fire Equipment (BAFE) approved to BS EN3⁹ or an EC or EEA standard, which in practice provides standards equivalent to those standards contained in a British standard, a valid certificate¹⁰
- a '1 to 10' employee CE approved¹¹ first aid kit, maintained and the contents within their 'use-by-date'.

Job specific reference information

2.25 An escort vehicle shall carry the following information:

- a copy of the route to be followed, consents and restrictions as appropriate
- appropriate maps
- contact telephone numbers for police/highway authority control rooms
- up to date traffic and road work information if applicable.

Communications equipment

2.26 An escort vehicle shall have a:

- communications link to the abnormal load or abnormal vehicle

⁶ British Standard 873 – Specification for Traffic Cones and Cylinders.

⁷ Schedule 12 of TSRGD (Traffic Sign Regulations and General Directions 2002 (SI 3113).

⁸ British Standard 3143 – Specification for Road Danger Lamps.

⁹ British Standard EN3 – Portable Fire Extinguishers.

¹⁰ British Standard 5306 – Maintenance of Portable Fire Extinguishers and British Standard 6643 – Recharging Fire Extinguishers apply, or an EC or EEA equivalent.

¹¹ CE Approved means compliance with European Standards.

- communications link to the police/highway authority control rooms (and emergency services if required).

2.27 A direct radio voice link shall be installed in the escort vehicle to enable communications between the escort vehicles and the abnormal load¹². Either a short-range (under 3km) two-way radio system¹³ shall be used or a long-range (3–8km) radio system accompanied by the appropriate radio licence. The use of hand-held microphones for 2-way radio equipment when driving is not prohibited by the regulations¹⁴ dealing with the use of hand-held mobile phones or similar devices when driving. However there is still a risk of distraction when using a microphone (whether hand-held or hands-free). Consequently, drivers continue to risk prosecution under existing powers dealing with the requirement to have proper control of a vehicle.

2.28 Hand-held mobile phones or similar devices shall not be used by the escort person whilst driving. A mobile phone or other device is defined as 'hand-held' if it is, or must be, held at some point during the course of making or receiving a call (or text, or email, or other data – eg to/from a GPS navigation device). 'Driving' will include pausing at traffic lights or in temporary traffic jams. Hands free equipment is not prohibited. If installed, then the system should be to the standard of EC Recommendation L32/200¹⁵ (<http://eur-lex.europa.eu/JOHtml.do?uri=OJ:L:2007:032:SOM:EN:HTML>). Note however the warning about the risk of distraction and prosecution in the paragraph above, which applies equally to hands-free mobile phones.

3 Escort person specification

3.1 It is the responsibility of hauliers to satisfy themselves that their own escort personnel are sufficiently competent to undertake the task of escorting abnormal loads. Competent means that they are suitably trained and/or experienced for the role. Where a subcontractor is employed the subcontractor shall ensure that their staff are competent, through training and/or experience for the role.

Escort person

Main duties and responsibilities

3.2 The main duties and responsibilities of an escort person are:

- to escort the abnormal load along the approved transport route acting as a warning to other road users and pedestrians
- to ensure that any special instructions/restrictions, for example cautions applying to bridges, tunnels or level crossings, either for the escort vehicle or the vehicle carrying the abnormal load, are adhered to

¹² This type of equipment can be used whilst driving.

¹³ Short range radio systems are readily available and can be used without the need for a registered frequency licence.

¹⁴ The Road Vehicles (Construction and Use) (Amendment)(No 4) Regulations 2003 SI 2695.

¹⁵ EC Recommendation L32/200 on safe and efficient in-vehicle information and communication systems.

- to act as a point of contact and to be the communications interface between the abnormal load convoy and police/highway authority control rooms, bridge authorities and emergency services if required
- to alert the emergency services as appropriate in the event of an incident involving the abnormal load. (If suitably trained, use the first aid kit and fire extinguisher as appropriate.)
- to place traffic cones and warning beacons around the abnormal load when it is stationary for a period of time in order to warn other road users
- to ensure that the escort vehicle is 'fit for purpose' and conforms to this code of practice.

Essential skills and qualifications

3.3 Essential skills and qualifications required are:

- a minimum age of 21 years old
- a full standard driving licence held
- to be able to speak and read the English language¹⁶
- to be competent through training and/or experience in the movement and transportation of abnormal loads or abnormal vehicles.

Desirable skills and qualifications

3.4 Desirable skills and qualifications are:

- basic first aid training and qualification¹⁷
- basic use of a fire extinguisher training¹⁸.

Personal protective equipment

3.5 Each escort person actively involved in escorting an abnormal load shall be equipped with a long sleeve high visibility fluorescent yellow jacket conforming to BS EN 471¹⁹ or an EC or EEA standard, which in practice provides standards equivalent to those standards contained in a British standard. This shall be worn at all times when the escort person is outside of the escort vehicle²⁰.

¹⁶ Alternatively, the escort person may be accompanied by a colleague, with whom they can communicate fully, who can speak and read English on their behalf.

¹⁷ For example – St John's Ambulance 1 day certified course – 'Lifesaver First Aid', or equivalent.

¹⁸ For example – Typical Fire Service ½ day Basic Fire Awareness and Hands on Training Course, or equivalent.

¹⁹ British Standard BS EN 471 – Specification for High Visibility Warning Clothing.

²⁰ Those undertaking self escorting operations should also consider the use and/or availability of other personal protective equipment considered as necessary.

Self certification

- 3.6 The haulier or subcontractor shall ensure that their self escort vehicles comply with this code of practice and that their personnel have sufficient training and/or experience to carry out self escorting duties competently and safely in a manner which accords with the requirements of Road Traffic and Health and Safety legislation²¹.
- 3.7 When notifying the movement of an abnormal load to the police, they may ask for a self-certificate or other evidence to show that the vehicles and personnel to be used comply with this code of practice. A self escorting declaration form is available at Appendix 2.

4 Operation

- 4.1 This section covers the general requirements and operating practices that shall be conformed to when actively engaged in self escorting an abnormal load. It also gives general guidance on the minimum threshold above which self escorting is required.
- 4.2 Some abnormal loads need an attendant, as defined in the relevant legislation²². An escort person can also carry out this role, provided they are not travelling in the vehicle carrying the abnormal load, and provided they fully meet the legislative requirements²³. Any person driving a vehicle for the purpose of assisting the propulsion of another vehicle cannot be the attendant.
- 4.3 The police abnormal loads officer and haulier shall agree on a case-by-case basis the minimum number of escort vehicles and escort personnel required for the movement.

Escorting criteria

Operating guidance

- 4.4 As general guidance the following dimensions may be applied to decide if the movement of an abnormal load requires an escort. Generally an escort vehicle is required if:

Measurement	For Motorways	For all other roads
Load / vehicle width exceeds	4.6m	4.1m
Overall vehicle length	-	30.5m

²¹ The Health & Safety at Work etc Act 1974, Chapter 37, The Management of Health and Safety at Work Regulations 1999 (SI 3242) and 'Driving at Work – Managing Work-Related Road Safety' which are Department for Transport (DfT) and Health & Safety Executive (HSE) guidelines available from the HSE website – www.hse.gov.uk

²² The Road Vehicles (Construction & Use Regulations) 1986 (SI 1078), Schedule 12 paragraph 2. Part 2 of The Road Vehicles (Authorisation of Special Types) (General) Order 2003 (SI 1998)

²³ Schedule 6 of The Road Vehicles (Authorisation of Special Types) (General) Order 2003 (SI 1998).

exceeds		
Vehicle gross weight exceeds	130 tonnes	100 tonnes

- 4.5 The above measures are for guidance and may differ between individual police forces who may wish to vary these requirements. The final decision rests with the police, along with timing of the move on specific stretches of road.
- 4.6 Depending on the situation or location, a police escort may be requested for all or part of the route. These requests will be made by the local police abnormal load officer when an application to move an abnormal load is made.
- 4.7 Upon notification police authorities will respond to the haulier when an escort is required and whether a police escort is necessary.
- 4.8 A combination of self escort and police escort vehicles may be used, to be agreed by both parties.
- 4.9 A haulier can choose to self escort an abnormal load below these guidelines.
- 4.10 The vehicle carrying the abnormal load shall not divert from the agreed route, unless directed to do so by police and only then when the appropriate highway and/or bridge authorities' approval has been obtained.
- 4.11 Drivers of the escort vehicle shall not contravene road traffic legislation.
- 4.12 Where self escort vehicles are changed along the route the appropriate paperwork and information shall be handed over.

5 Before starting the journey

- 5.1 It is important that the designated escort driver knows exactly what is expected of them, either from their own company, or from their client before undertaking the job.

Escort vehicle check

- 5.2 In addition to familiarising themselves with the load and the proposed route, the escort driver should ensure that the escort vehicle(s) are fit for purpose.

Pre-journey checks with abnormal load or abnormal vehicle personnel

- 5.3 It is vital that the escort driver and the driver of the abnormal load liaise prior to the journey, either on the day or in advance. Before meeting up with the vehicle(s) to be escorted, familiarisation with the local road layout and of how to exit the departure point will be useful.
- 5.4 The notified route to be taken should be communicated to the escort driver (ideally in advance). Any disagreement or uncertainty must be resolved prior to departure. The route can only be the one notified to the relevant authorities and may have specific

instructions associated with it eg regarding timings or negotiation of particular structures. Only the notified authorities can agree any changes to the route.

- 5.5 A route plan should be agreed, including a timetable for the journey and detailing changeover or lay-up points and how specific route cautions will be complied with. The start time should give due regard to peak-traffic levels and any local requirements.
- 5.6 If the police are assisting with the escort for all or part of the journey, follow any police instructions given.
- 5.7 If the escort driver is aware of any obvious problem(s) with the load or vehicle, then they should be made known to the driver of the load.
- 5.8 The escort driver shall contact each relevant police force traffic control room 30 minutes prior to arrival in that police area or before departing from a location within that police area, quoting any reference or authorisation numbers that apply. Any police guidance relating to traffic or weather conditions should be followed. On arrival at a destination or when leaving a police force area the relevant traffic control room shall again be notified.
- 5.9 The escort driver should also check whether there are requests to inform journey details²⁴ to:
 - **National Traffic Control Centre (Tel: 0121 245 4688 - 24hrs)** if using motorways and trunk roads in England
 - **Traffic Wales (Tel: 0845 602 6020 - 24hrs)** if travelling on Welsh trunk roads
 - **Traffic Scotland (Tel: 0141 287 9283 – 24hrs)** if using motorways and trunk roads in Scotland.

6 On the road

- 6.1 The escort vehicle's function is to warn other road users, including pedestrians of the presence of an abnormal load, as well as to maintain ongoing contact with the driver of the abnormal load. Whenever escort duties are being performed the communications link between the escort vehicle and the load should not be broken.
- 6.2 On motorways and dual carriageways, an escort vehicle should be positioned to the rear of the abnormal load, at a distance to give adequate warning to other road users of the load.
- 6.3 On two-way roads and at traffic islands or intersections, an escort vehicle shall be positioned to the front of the abnormal load to give adequate warning to other road users of the load.

²⁴ In particular, this may apply for Special Order loads (loads in excess of 6.1m wide, 30m long (rigid), 150,000kgs gross weight and/or with one or more axles exceeding 16,500kg) and VR1 loads (loads in excess of 5m wide). There may also be a requirement to update such organisations whilst en route.

Basic principles on vehicle manoeuvres

6.4 The following are a number of general statements of best practice, but each load and route will dictate the best way to escort.

Manoeuvre to close a lane to the left

6.5 A motorist may attempt to undertake the convoy, particularly near to entrance/exit slip roads; if this happens, the following actions should be taken:

- the driver of the load should be warned that a vehicle is undertaking to the left
- if an opportunity for undertaking presents itself to other traffic, the escort driver should safely move into the left adjacent lane²⁵
- the driver of the load should be told that the left lane has been secured
- the distance between the escort vehicle and the load should be shortened
- the normal escorting position and distance should be resumed when appropriate.

Manoeuvre to close a lane to the right

6.6 The driver of the load may inform the escort driver that they wish to move out eg because of an obstruction on the hard shoulder; to do this safely the following actions should be taken:

- the escort driver should mirror, signal, manoeuvre into the right adjacent lane when there is sufficient space and with regard to the speed of other traffic
- the escort driver should inform the driver of the load that the right lane is secured
- the distance between the escort vehicle and the load should be shortened
- once clear of the obstruction, the escort driver should indicate left and resume normal escort position
- the driver of the load should be told it is safe to move back into the left lane.

Procedure for entering a motorway (principally for slow wide loads)

6.7 To join a motorway safely the following actions should be taken:

- the escort vehicle should enter the motorway in front of the load
- from the slip road, the escort driver should ensure that the hard shoulder is clear – special care should be taken when entering from short slip roads
- the escort vehicle should be positioned in the right hand lane, with the load in left hand lane

²⁵ Unless the left adjacent lane is the hard shoulder.

- the escort vehicle should enter lane 1 before the load enters the motorway, ensuring that no vehicles can get between the escort vehicle and the load
- the driver of the load should be advised that lane 1 is secured
- if, due to the width of the load, lane 2 also needs to be secured, the manoeuvre to close the right lane should be followed
- once the load is on the motorway the normal escort position should be resumed. If it is not possible to carry out this action safely an additional escort vehicle should be employed.

Procedure for leaving a motorway

6.8 To leave a motorway safely the escort vehicle should prepare for the convoy to leave at the next junction by taking the following actions:

- the escort driver should indicate left in good time, but not move left straight away
- the distance between the escort vehicle and the load should be shortened
- the driver of the load should be advised to enter the slip-road, and warned if vehicles are attempting to undertake
- once the load begins to progress up the slip-road, the indicator should be cancelled and the distance between the escort vehicle and the abnormal load increased
- if transferring to a single-carriageway road, traffic island or road junction, the escort vehicle should be prepared to move to the front prior to entering the slip road if necessary.

Basic principles – escorting from the rear

6.9 The following are a number of general statements of best practice, but each load and route will dictate the best way to escort.

- Maintain communication with driver of the load, and agree how manoeuvres will be communicated eg what will be said when the driver of the load wishes the escort driver to secure a second lane.
- Contact police/highway control rooms 30 minutes in advance, as necessary, as route progresses.
- When on dual carriageways and motorways, position the vehicle at a suitable distance behind the load (approximately 50-75m).
- This distance could be varied depending on road, traffic and/or weather conditions.
- The positioning of the escort vehicle on the road should be guided by the width of the load:
 - If the load only takes lane 1, position the escort vehicle in lane 1 behind, ensuring as far as possible that the escort vehicle does not obscure beacons on the load.

- If the load encroaches into lane 2, it may be appropriate to position the escort vehicle in lane 2, so the escort vehicle protects the full width of the load at all times.
- Give other motorists the maximum warning of any manoeuvre, which should always be gradual and smooth.
- When escorting, considerable traffic build-up can occur behind the load. Opportunities to pull the load over safely and let the traffic pass should be taken at suitable intervals.

Basic principles – escorting from the front

6.10 The following are a number of general statements of best practice, but each load and route will dictate the best way to escort.

- Escort drivers must always position their vehicle at the front of the load at the start of an escort, on single carriageway 2-way roads and at traffic islands or intersections. The primary purpose when leading the load is to warn other road users and pedestrians of the presence and potential danger of the oncoming abnormal load. It is also to alert the driver of the load to potential hazards both on the road and overhead.
- The escort vehicle should be at an appropriate distance ahead of the load to give oncoming traffic the maximum amount of warning. If possible, visual contact should be maintained with the load but this will not be the case when the escort vehicle has negotiated one or a number of bends ahead of the load.
- Contact police/highway control rooms 30 minutes in advance, as necessary, as route progresses.
- The driver of the vehicle being escorted should be alerted to parked vehicles, pedestrians, obstructions and vehicles that may have pulled out behind the escort vehicle.
- The escort vehicle should be positioned as far to the right as possible without straddling the centre line, with dipped headlights on. Other vehicles should be signalled to slow down or move over to their nearside as necessary. Any hand actions²⁶ need to be slow and deliberate and reflective jackets should be worn to improve visibility.
- When escorting, considerable traffic build-up can occur behind the load. Opportunities to pull the load over safely and let the traffic pass should be taken at suitable intervals.

In the event of a breakdown of the abnormal load vehicle

6.11 The nature of the breakdown should be quickly established from the driver of the load, and whether they are able to continue, at least to a suitable stopping area, without deviating from the agreed route. If unable, any deviation from the route **must** be

²⁶ Use the signals set out in the Highway Code

cleared with the relevant authorities (this includes, for example, passing over structures to get to motorway service areas).

- 6.12 The escort vehicle should be placed at an appropriate distance and in the most advantageous position to protect the load and its personnel, using the lights and markings to give the maximum warning to other traffic. There is no definitive guide to an escort vehicle's orientation in such circumstances; the nature and position of the breakdown will dictate. However, the escort vehicle should be left with its front wheels steered in-line, and in the case of an automatic gearbox, with the gear selector in park. The handbrake must also be set.
- 6.13 The police and/or highway control room should be contacted without delay, especially if the broken down vehicle is causing an obstruction. All instructions given should be followed.
- 6.14 If the escort vehicle breaks down, the driver of the abnormal load should be informed immediately. Both the escort vehicle and the abnormal load should be parked in, or moved to, a safe place. The escort driver should make arrangements for repairs or a replacement as soon as possible, and the police informed if necessary.
- 6.15 Cones should only be placed to warn others of an obstruction if the drivers of the load and the escort agree that it is safe to do so, having taken account of the circumstances of the breakdown and the risks posed to themselves and other road users.

7 Parking up

- 7.1 There is no definitive guide to the recommended orientation of an escort vehicle when the abnormal load is stationary in a live traffic situation. The section describes the three main choices and the advantages and disadvantages of each. The escort driver should decide which is most appropriate for the circumstances.

Vehicle parked "In line"

- 7.2 The escort vehicle is positioned behind the abnormal load facing forwards.

The advantages are:

- rear facing amber bar lights and red brake lights are most visible
- rear vehicle markings most visible
- driver of vehicle is not exposed to direct impact on exiting vehicle
- it can prevent obstructing the live carriageway if the hard shoulder or lay-by is particularly narrow.

The disadvantages are:

- may appear to approaching drivers that the vehicle is moving
- does not fill the lane, therefore offering only a limited line of defence

- if the vehicle is struck it will travel forward and may strike the load.

Vehicle parked in "Fend off" position

7.3 The escort vehicle is positioned behind the abnormal load facing forwards and to the right.

The advantages are:

- fills the lane and therefore provides a line of defence
- approaching drivers tend to want to pass by driving around the front
- provides a forewarning that the vehicle is stationary
- visually the vehicle tends to get larger as approaching vehicles draw nearer
- when parked on a motorway hard shoulder, the vehicle tends to encourage people away from it, since it appears as though it could be emerging from the hard shoulder and is more likely to arouse caution from an approaching motorist
- provides the escort driver with a better view of approaching traffic when looking to exit the vehicle
- provides a better view of approaching traffic over the escort vehicle's bonnet, and enables the escort driver to be better aware of approaching traffic.

The disadvantages are:

- rear lights aimed towards the hard shoulder
- when parked on a hard shoulder approaching motorists may be inclined to react by slowing or changing lanes having wrongly assumed that the escort vehicle is emerging from the hard shoulder
- escort driver is exposed to a direct impact from an errant vehicle
- when on the hard shoulder, if struck, the vehicle is more likely to be projected into live lanes; an errant vehicle having struck the escort vehicle may then breach the zone between escort vehicle and load.

Vehicle parked in "Fend in" position

7.4 The escort vehicle is positioned behind the abnormal load facing forwards and to the left.

The advantages are:

- fills the lane and therefore provides a line of defence
- provides a forewarning that the vehicle is stationary
- driver not exposed to direct impact on exiting vehicle
- when parked on the hard shoulder, if struck, it will minimise the likelihood of the vehicle being projected into the live carriageway.

The disadvantages are:

- rear lights are aimed towards the central reservation
- when "fend in" is used whilst parked on the hard shoulder, and a nearside barrier or wall is present, if the vehicle is struck it is more likely to collide with the barrier and then continue to travel along the hard shoulder alongside the barrier and towards the load
- visually the vehicle tends to get smaller as approaching vehicles draw nearer
- with the vehicle in the "fend in" position it tends to obstruct the escort driver's view of approaching traffic when returning to the vehicle
- approaching drivers tend to want to pass by driving around the front.

8 Job completed

- 8.1 If the layout of the destination is unfamiliar to the escort driver, or is served by minor or locally busy roads, it is recommended that the load is parked at a suitable stopping point before a survey is undertaken of approach roads. Any specific access requirements should be agreed with site managers.
- 8.2 On the approach to the destination, the escort vehicle should be positioned in front of the load to warn any vehicle or person leaving the site of the presence of the load.

Appendix 1: References

Reference	Organisation
Code of Practice Self escorting of Abnormal Loads and Abnormal Vehicles (PR210/07)	Highways Agency (HA)
Operating Guidance for Abnormal Load Escorting (PR210/07)	HA
Escorting Method Statement	Abnormal Load Escort Network
Escorting Manual	Heavy Transport Association
Highway Code	Department for Transport
Highways Agency Traffic Officer Manual	HA

Appendix 2: Self escorting declaration

Self escorting declaration

I, the undersigned, certify that:

- the following vehicle(s) meet the vehicle specification contained within the Lighting and Marking code of practice for Abnormal Load self escorting vehicles
- the driver(s) of the vehicle(s) meet the essential criteria and operating guidance contained within the aforementioned code of practice .

Registration number	Vehicle type

Signature	Name (block capitals)	Status in company	Name of company

Appendix 3: Abnormal load classifications and notifications

Gross vehicle weight	Axle weight	Width ≤2.9m Length ≤18.65m	W>2.9m L>18.65 m	W>3m L>18.75m Vehicle combination L>25.9m	W>5m, VR1	W>6.1m L>30m, SO
≤18,000kgs (2-axle) ≤26,000kgs (3-axle) ≤32,000kgs (4-axle rigid) ≤36,000kgs (2+2 axle artic) ≤40,000kgs (5-axle) ≤44,000kgs (others)	C&U AWR limits	N/A	Police: 2 days	Police: 2 days	HA AIL team: 2 weeks Police: 2 days	HA AIL team: 10 weeks
Exceeding above limits or ≤ 50,000kgs, STGO Cat 1	AWR limits	Road and Bridge Authorities (RBA): 2 days	RBA: 2 days		HA AIL team: 2 weeks Police: 2 days RBA: 2 days	Police: 5 days RBA: 5 days
>50,000kgs, STGO Cat 2	≤12,000kgs					
>80,000kgs, STGO Cat 3	≤16,500kgs	Police: 2 days RBA: 5 days	Police: 2 days RBA: 5 days		HA AIL team: 2weeks Police: 2days RBA: 5days	
>150,000kgs, Special Order (SO)	>16,500kgs	HA AIL team: 10 weeks Police: 5 days RBA: 5 days				

Abnormal indivisible load categories and notifications

C&U <ul style="list-style-type: none"> Loads in excess of 2.9m wide or 18.65m long: 2 clear days notice to Police 	STGO <ul style="list-style-type: none"> Loads greater than 50,000kgs or C&U limits and less than 80,000kgs: 2 clear days notice and indemnity to RBA Loads over 80,000kgs: 2 clear days notice to Police and 5 clear days RBA notification with indemnity
STGO VR1 <ul style="list-style-type: none"> Application to HA 2 weeks prior to proposed date of movement Loads greater than 44,000kgs or C&U limits and less than 80,000kgs: 2 clear days notice to Police and RBA notification with indemnity Loads over 80,000kgs: 2 clear days notice to Police and 5 clear days RBA notification with indemnity 	Special Order (SO) <ul style="list-style-type: none"> Application to HA 10 weeks prior to move 5 clear days notice to Police and RBA notification with indemnity

Key to classifications

Note: There are other factors, eg rigid length and overhangs that also affect the notification requirements. There are also Authorised Weight Regulations that may apply.

Appendix 4: Organisational endorsement

This document is endorsed by the following organisations:

Association of Chief Police Officers



Association of Chief Police Officers in Scotland



Construction Plant-hire Association



Department for Transport



Freight Transport Association



Heavy Transport Association



Road Haulage Association



If you need help using this or any other Highways Agency information please call **0300 123 5000** and we will assist you.

© Crown copyright (2012).

You may re-use this information (not including logos) free of charge in any format or medium, under the terms of the Open Government Licence. To view this licence: visit <http://www.nationalarchives.gov.uk/doc/open-government-licence/> write to the Information Policy Team, The National Archives, Kew, London TW9 4DU, or email psi@nationalarchives.gsi.gov.uk.

This document is also available on our website at www.highways.gov.uk

If you have any enquiries about this publication email ha_info@highways.gsi.gov.uk or call 0300 123 5000.

Please quote the Highways Agency publications code PR22/12.

Highways Agency media services Birmingham. Job number M1200128

Directgov

Public services all in one place
www.direct.gov.uk

Aide Memoire for notification requirements for the movement of Abnormal Indivisible Loads or vehicles by road when not complying with The Road Vehicles (Construction and Use) Regulations 1986 (commonly known as C & U)

Weight

Gross weight of vehicle carrying the load exceeding C & U limits up to 80,000kgs (78.74 tons)	2 clear days notice with indemnity to Road and Bridge Authorities.
Gross weight of vehicle carrying the load exceeding 80,000kgs up to 150,000kgs (147.63 tons)	2 clear days notice to Police and 5 clear days with indemnity to Road and Bridge Authorities.
Gross weight of vehicle carrying the load exceeding 150,000kgs (147.63 tons)	Highways England Special Order* plus 5 clear days notice to Police and 5 clear days notice with indemnity to Road and Bridge Authorities

Width

C & U loads:- width exceeding 2.9m (9ft 6ins) up to 4.3m (14ft 1 ins)	2 clear days notice to Police
STGO loads:- width exceeding 3.0m (9ft 10ins) up to 5.0m (16ft 5ins)	
Width exceeding 5.0m (16ft 5ins) up to 6.1m (20ft)	Highways England form VR1** plus 2 clear days notice to Police
Width exceeding 6.1m (20ft)	Highways England Special Order* plus 5 clear days notice to Police and 5 clear days notice with indemnity to Road and Bridge Authorities

Length

C&U loads:- length exceeding 18.65m (61ft 2in) up to 27.4m (90ft) - See C&U Regulations 1986 for definition of length	2 clear days notice to Police
STGO loads:- length exceeding 18.75m (61ft 6 ins) - See part 2, article 12 of the Road Vehicles (Authorisation of Special Types) (General) Order 2003 (Commonly known as STGO) for definition of length	
Overall length of a part 2 vehicle-combination exceeding 25.9m (85ft)	2 clear days notice to Police
Maximum length exceeding 30.0m (98ft 5ins) - see STGO Schedule 1, part 4, paragraph 25 for definition of maximum length	Highways England Special Order* plus 5 clear days notice to Police and 5 clear days notice with indemnity to Road and Bridge Authorities.
NB For some very light loads, such as yacht masts, that are moved on conventional motor vehicles not exceeding 12 tonnes gross weight or trailers not exceeding 10 tonnes gross weight, a Highways England Special Order* will be required if the rigid length exceeds 27.4m (89ft 11ins)	

NOTE 1 "Clear days Notice" excludes Saturdays, Sundays or a public holiday in any part of Great Britain in relation to movements authorised by the Special Types General Order only, there being no such exclusion in Special Orders unless specifically stated.

NOTE 2 There is no statutory limit governing the overall height of a load, however, when applying for a Special Order or VR1 it should, wherever possible, not exceed 4.95m (16ft 3ins) in order that the maximum use can be made of the motorway and trunk road network.

NOTE 3 The notification requirements for mobile cranes can be found in the Road Vehicles (Authorisation of Special Types) (General) Order 2003, statutory instrument number 1998 (Part 2 Articles 10 to 18), which is available on the OPSI website:
<http://www.legislation.gov.uk/uksi/2003/1998/contents/made>

NOTE 4 Application to move Special Types or Special Purpose vehicles, such as very large agricultural vehicles, that may not be fully permitted by the Construction & Use (C&U) Regulations or fall outside the scope of the Special Types General Order should be made to the Vehicle Certification Agency (VCA). Their website is at <http://www.dft.gov.uk/vca/>

*A Special Order application can be completed and submitted online at www.highways.gov.uk/esdal. The Special Order application form BE16 can also be downloaded and e-mailed to the address below. Approval is not automatic and is at the discretion of the Highways England abnormal loads team acting on behalf of the Secretary of State for Transport. To ensure that the necessary clearances can be obtained in good time from the Police, Highway and Bridge Authorities, you should request permission for the move by returning the completed form 10 weeks prior to the scheduled date of the move. In fact you cannot apply too early and we invite manufacturers or hauliers to contact us at pre tender stage, before making a financial commitment to supply the load, to check whether permission would be granted.

** A VR1 application can be completed and submitted online at www.highways.gov.uk/esdal. The form can also be downloaded but must not be e-mailed or faxed because the VR1 form is a legal document and so we must receive the original signed form. Approval is not automatic and is at the discretion of the Highways England abnormal loads team acting on behalf of The Secretary of State for Transport. To ensure that the necessary formalities can be completed in good time, you should request permission for the move by posting the completed form 2 weeks prior to the date of the scheduled move. Again, you cannot apply too early and we invite manufacturers or hauliers to contact us at pre tender stage, before making a financial commitment to supply the load, to check whether permission would be granted.

Forms and enquiries to:
Highways England
Abnormal loads team
9th Floor, The Cube
199 Wharfside Street
Birmingham B1 1RN

E-mail: abnormal.loads@highwaysengland.co.uk
Tel: 0300 470 3004

TECHNICAL INFORMATION SHEET

Axle weights and load distribution

1. Introduction

This Technical Information Sheet provides guidance on where to position loads on standard 13.6 m (45') long trailers in order to satisfy legal limits for axle weights.

Note: There are many variables that affect the permissible axle loads on a vehicle:

- Using 2 or 3 axle tractor units
- Tractor axle kerb weights
- Fuel load
- Axle design limits
- Tractor unit axle spacing
- Suspension design
- 5th wheel position
- Trailer length
- Trailer tare weights on king-pin and tri-axle
- Position of king-pin from headboard
- Distance between king-pin and tri-axle
- Tri-axle spacing
- Trailer suspension type - mechanical or air suspension

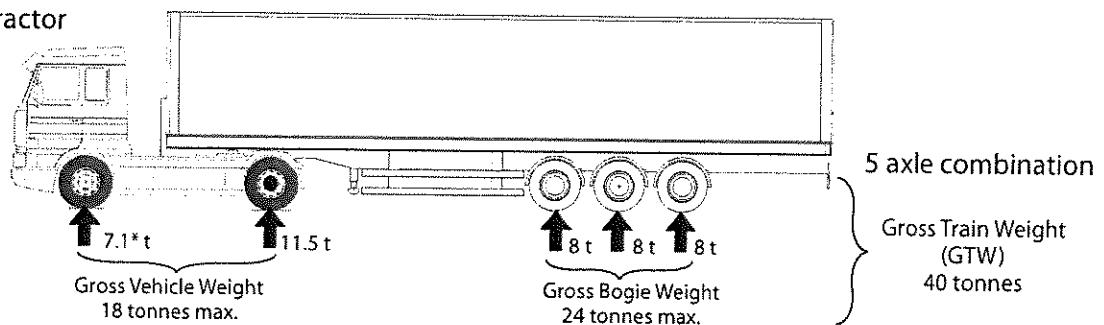


Therefore, this Technical Information Sheet can only provide guidance for **typical** tractor - trailer combinations. **Important note: The final responsibility for meeting legal limits lies with the driver of the vehicle being used.**

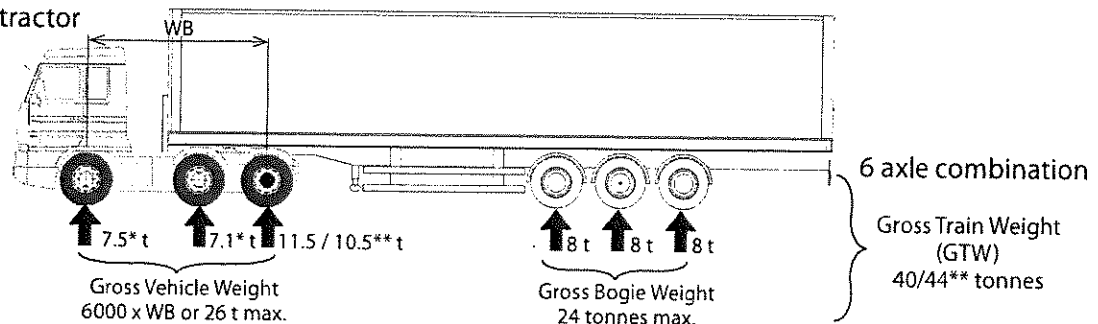
2. Axle weight limits

Axle weight limits vary from country to country and a list of permissible maximum weight limits for the main European destinations can be found in Table 1 on page 4. **Typical** axle weight limits for European freight are shown below:

Two axle tractor



Three axle tractor



* Steer axle and 2nd axle Authorised Weights are make/model specific.

** In the UK Drive axle limit is 10.5 t if GTW exceeds 40 tonnes.

Note: The weight borne by the drive axle must not be less than 25 % of the GTW for international traffic.

TECHNICAL INFORMATION SHEET

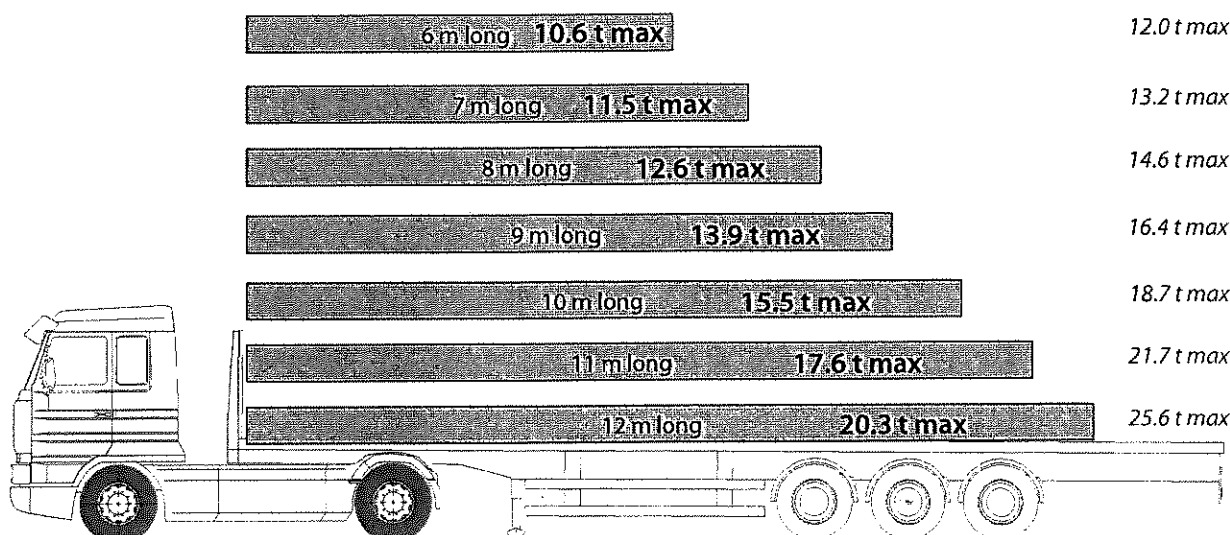
Axle weights and load distribution

3. Maximum payload when loaded to the trailer headboard

The diagrams below show the maximum payload for different length product when loaded to the trailer headboard based on *typical* 2 and 3 axle tractor units and trailers. The figures below and in Section 4 are based on vehicle tare weights of 14.8 tonnes for a 5 axle combination, and 15.9 tonnes for a 6 axle combination with 13.6 m (45') trailers.

3.1 Two axle tractor unit / 5 axle combination

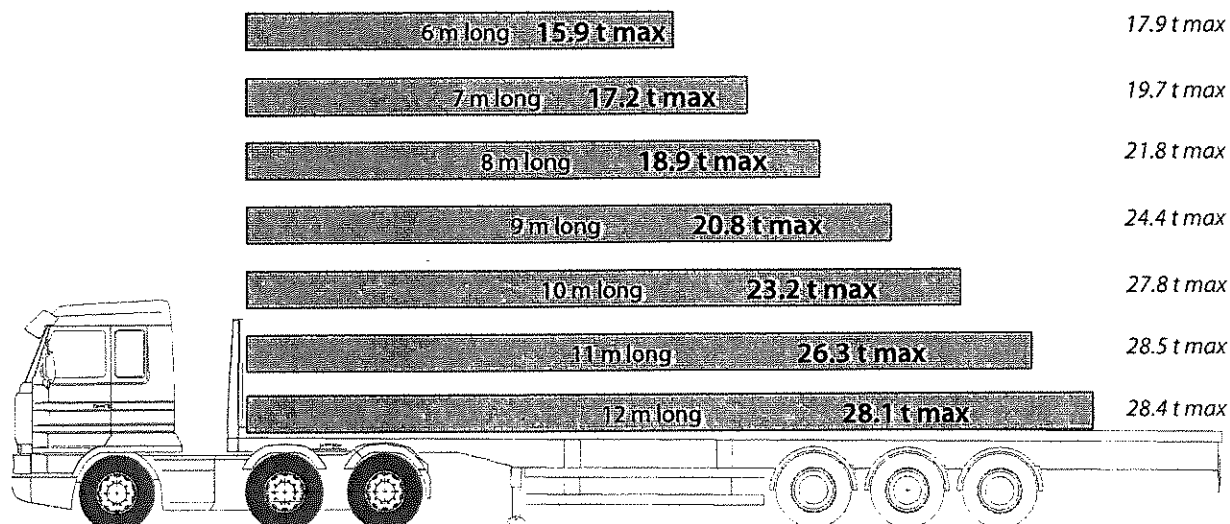
*Weight limits for a
12.2 m (40') trailer*



Maximum payload for different length product loaded to the headboard with a *typical* 2 axle tractor unit and a 13.6 m (45') trailer

3.2 Three axle tractor unit / 6 axle combination

*Weight limits for a
12.2 m (40') trailer*



Maximum payload for different length product loaded to the headboard with a *typical* 3 axle tractor unit and a 13.6 m (45') trailer



1. The above figures are for guidance only based on *typical* tractor and trailer tare weights and dimensions.
2. When loading 3 axle tractor units in the UK for delivery to Mainland Europe, the load should be positioned for a 2 axle unit unless it is known for certain that a 3 axle tractor will be used on the continent.

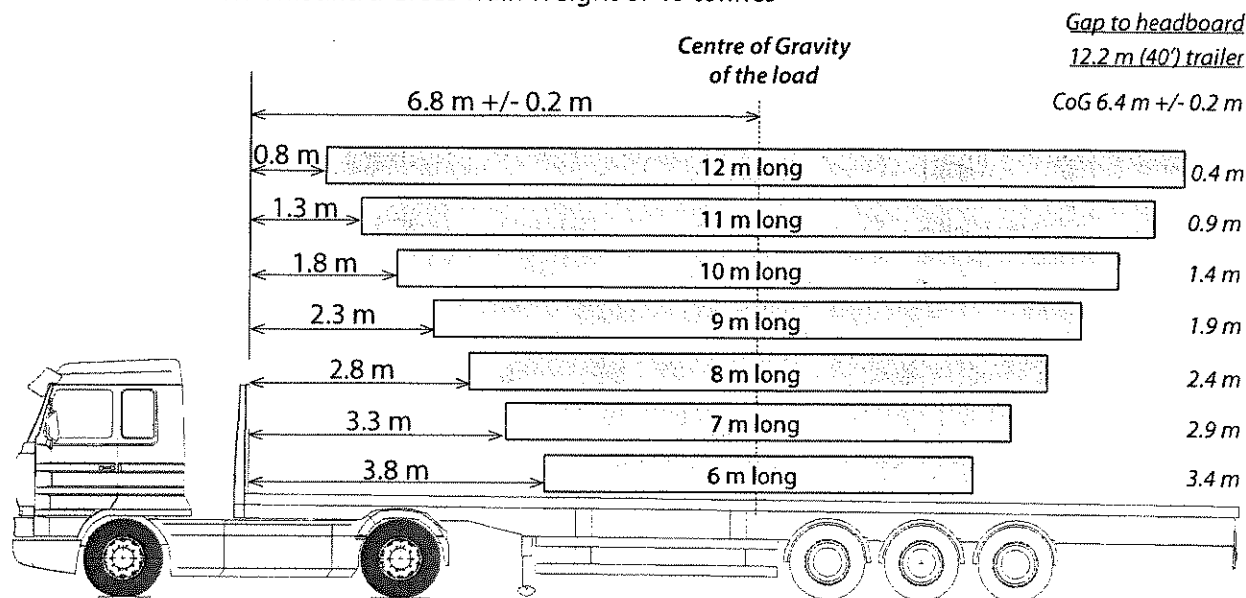
TECHNICAL INFORMATION SHEET

Axle weights and load distribution

4. Load position to meet axle limits with full payloads

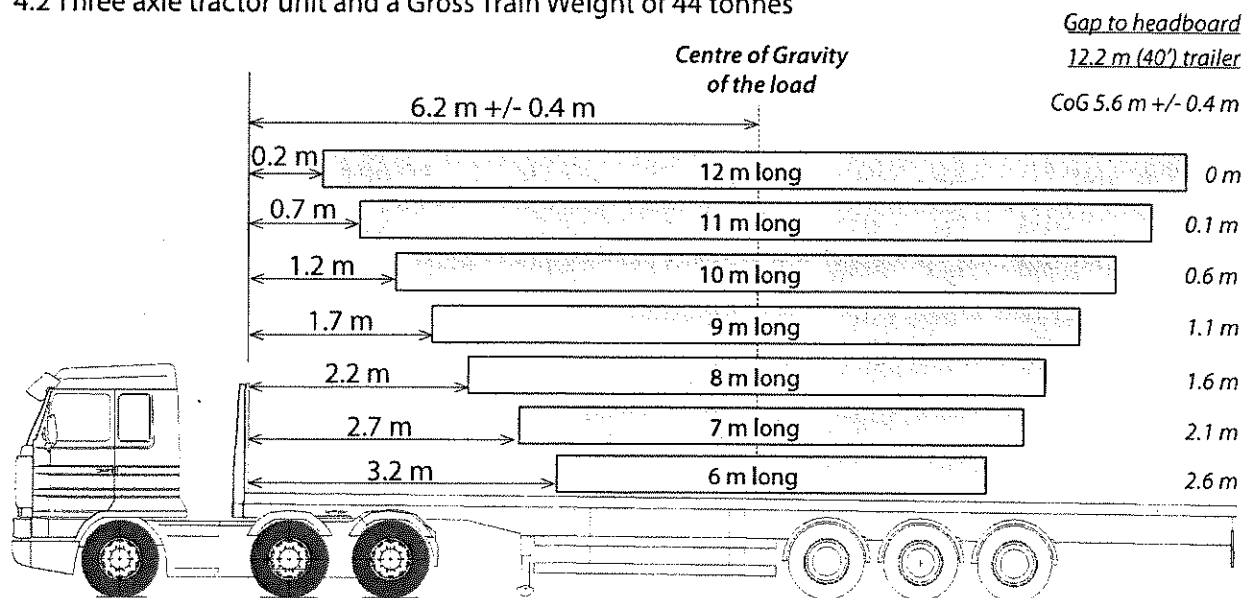
The diagrams below show the optimum position of the load to meet axle load limits when full payloads are carried. The allowable variation shown for the position of the Centre of Gravity also applies to the distance from the headboard i.e. ± 0.2 m for full payloads. Reducing the payload by 1 tonne adds approximately 0.1 m to the allowable variation.

4.1 Two axle tractor unit and a Gross Train Weight of 40 tonnes



Distance from the trailer headboard of a 25 tonne payload on a typical 5 axle vehicle with a 13.6 m (45') trailer.

4.2 Three axle tractor unit and a Gross Train Weight of 44 tonnes



Distance from the trailer headboard of a 28 tonne payload on a typical 6 axle vehicle with a 13.6 m (45') trailer.



1. The above figures are for guidance only based on **typical** tractor and trailer tare weights and dimensions.
2. When loading 3 axle tractor units in the UK for delivery to Mainland Europe, the load should be positioned for a 2 axle unit unless it is known for certain that a 3 axle tractor will be used on the continent.

TECHNICAL INFORMATION SHEET

Axle weights and load distribution

Table 1: Permissible maximum weights of 5 and 6 axle articulated trucks in Europe (in tonnes)

Country	Weight per non-drive axles	Weight per drive axle	Tractor 2 axle unit GVW	Tractor 3 axle unit GVW	Trailer tri-axle bogie	Gross train weight 5 axles	Gross train weight 6 axles	Container transport 5 / 6 axles
Albania (AL)	10	10 / 11.5 (1)	18	25 / 26 (1,2)	22	38 / 44 (1)	38 / 44 (1)	
Austria (A)	10	11.5	18	26 (2)	24	40	40	44
Belgium (B)	10	12	19	26	30	43 / 44 (3)	43 / 44 (3)	
Bosnia-Herzegovina (BIH)	10	11.5	20	26	26	40	40	
Bulgaria (BG)	10	11.5	18	26 (2)	24	40	40	44
Croatia (HR)	10	11.5	18	26 (2)	24	40	40	
Czech Republic (CZ)	10	11.5	18	26 (2)	24	42	48	48
Denmark (DK)	10	10 / 11.5 (1)	18	24 / 26 (1,2)	24	42	48	
Finland (FIN)	10	11.5	18	26 (2)	30	42	48	
France (F)	13/12 (2,4)	13 / 12 (2,4)	19	26	24	40 / 44 (5)	40 / 44 (5)	44
Germany (D)	10	11.5	18	26 (2)	24	40	40	44
Greece (GR)	7 / 10	11.5 / 13	18	26	24	40	40	44
Hungary (H)	10	11.5	18	26 (2)	24	40	40	44
Ireland (IRL)	10	10.5 / 11.5 (6)	17 / 18	26 (2)	24	40 / 42 (7)	44 / 46 (8)	44
Italy (I)	12	12	18	26 (2)	26	44	44	44
Netherlands (NL)	10	11.5	21.5	33	30	50	50	
Norway (N)	10	11.5	19.5	26 / 29.5 (9)	30	47	47	
Poland (PL)	10	11.5	18	26 (2)	24	40	40	44
Portugal (P)	10	12	19	26	24	40	40	44
Romania (RO)	10	11.5	18	26	24	40	40	44
Russia (RUS)	10	10	18	25 (2)		38	38	
Serbia (SRB)	10	11.5	18	24	22	40	40	44
Slovakia (SK)	10	11.5	18	26 (2)	24	40	40	
Slovenia (SLO)	10	11.5	18	25	24	40	40	44
Spain (E)	10	11.5	18	26	24	40	40	42 / 44
Sweden (S)	10	11.5	18	26 (2)		48 / 60 (10)	48 / 60 (10)	
Switzerland (CH)	10	11.5 (11)	18	26 (2)	24	40	40	
Turkey (TR)	10	11.5	18	25 / 26 (9)		40	40	44
Ukraine (UA)	10	11	16 / 18	22 / 24		38	38	44 / 46 (12)
United Kingdom (GB)	10	11.5 / 10.5 (2,4)	18	26 (13)	24	40	44	44

Notes

- (1) Lower figure is for national traffic; higher figure is for international traffic.
- (2) With road friendly (air) suspension or similar.
- (3) 43 t with mechanical suspension, 44 t with air suspension.
- (4) Drive axle must not exceed the lower figure for a 3 axle tractor unit operating above 40 t GTW.
- (5) 44 t limit for vehicles registered after 1 Oct 2009. As of 30 Sept 2014, also vehicles registered after 1 Oct 2001 (Euro III vehicles).
- (6) Mechanical suspension national traffic 10.5 t; air suspension national traffic 11.5 t; international traffic 11.5 t.
- (7) 42 t limit only until 31 Dec 2014 - then reverts to 40 t limit.
- (8) 46 t limit requires vehicle to have Electronic Braking System and Electronic Stability Control.
- (9) Certain national conditions apply.
- (10) For vehicles engaged in combined road/rail transport. In Sweden the higher value relates to 'B-doubles'.
- (11) Switzerland enforce a **minimum** drive axle load limit of 25% of the Gross Train Weight to ensure traction.
- (12) Licensed by state Motor Road service of Ukraine 5 axle + limit is 46 t.
- (13) 6 x Tractor Wheel Base (in m) capped at 26 tonnes. Must have road friendly suspension.

Sources: International Transport Forum 12 Oct 2011

IRU Maximum weights and dimensions 12 Mar 2012

Road Safety Authority Ireland Weights and Dimensions leaflet Feb 2013



When planning a load, the lowest limits of the countries on the whole route must be used.

Care has been taken to ensure that the contents of this publication are accurate, but Tata Steel Europe Limited and its subsidiaries do not accept responsibility or liability for errors or information that is found to be misleading.

Form VR1 (Revised 09/15)

Application for Authority to move a vehicle which, with load exceeds 5.0 metres but does not exceed 6.1 metres in width. All applicants must complete Part 1 and the supplement overleaf.

Emails, photocopies or faxes of this form are not acceptable. Amendments to this form are not acceptable. Errors must not be corrected by scoring through or using correcting fluid.

Part 1

1. Applicant's name and address.	2. Name and address of Haulier (if different from 1.)
3. Approximate date(s) of movement.	
4. Address from which journey will start.	
5. Address at which journey will end.	
6. Description of vehicle.	
7. Overall width of vehicle with load.	metres
Overall length of vehicle with load.	metres
Overall height of vehicle with load.	metres
Gross weight of vehicle with load.	kilograms
8. Nature and description of load with nett dimensions and weight.	
9. Number of separate pieces and number of loads this represents.	
Signature Date Phone Name (BLOCK CAPITALS) Fax	

Part 2 – Authority for movement

The Secretary of State hereby authorises under the provisions of the Motor Vehicles (Authorisation of Special Types) General Order 2003 the movement of the vehicle(s) and load(s) detailed above.
This Authority should not be taken in any way as relieving the haulier of any obligations under the Road Vehicles (Construction and Use) Regulations, the Motor Vehicles (Authorisation of Special Types) General Order 2003, or otherwise.

Attention is drawn to the safety precautions mentioned on VR1 supplement No. 2 attached.

Signed by Authority of the Secretary of State _____ Date _____

This actual Authority must be carried by the Driver of the vehicle on any journey authorised by it. Photocopies or faxes of this form are not acceptable.

Official Embossed Stamp

Supplementary Information

1. Total distance of the road movement (in miles).	
2. What is approximate value of load?	
3. Give date of authority and starting point of any similar movement previously authorised.	
4. Would division of the load to reduce width involve undue expense or risk of damage? If yes, state (a) the additional cost. (b) the nature of the risk.	Yes <input type="checkbox"/> No <input type="checkbox"/>
5. Has part sea shipment been considered? If yes, state (a) between which ports. (b) amount and source of quotation.	Yes <input type="checkbox"/> No <input type="checkbox"/>
6. Is address given at 5. overleaf the final destination? If no, give details of further proposed movements.	Yes <input type="checkbox"/> No <input type="checkbox"/>
7. What is approximate cost of road movement?	
8. Any additional consideration you wish taken into account should be mentioned here.	
<i>Send this form to:-</i> Highways England Abnormal loads team 9 th Floor, The Cube 199, Wharfedale Street Birmingham B1 1RN	<i>For journeys originating in Scotland send this form to:-</i> Transport Scotland Trunk Road Network Management Division Admin Team Buchanan House 8 th Floor, 58 Port Dundas Road Glasgow G4 0HF

For Office Use Only

Risk Assessment

All employers must conduct a risk assessment. If you have fewer than five employees you don't have to write anything down.

We have started off the risk assessment for you by including a sample entry for a common hazard to illustrate what is expected (the sample entry is taken from an office-based business). Look at how this might apply to your business, continue by identifying the hazards that are the real priorities in your case and complete the table to suit. You can print and save this template so you can easily review and update the information as and when required. You may find our example risk assessments a useful guide (<http://www.hse.gov.uk/risk/casestudies>). Simply choose the example closest to your business.

Company name:

Date of risk assessment:

What are the hazards?	Who might be harmed and how?	What are you already doing?	Do you need to do anything else to control this risk?	Action by who?	Action by when?	Done
Slips and trips	Staff and visitors may be injured if they trip over objects or slip on spillages.	General good housekeeping is carried out. All areas well lit, including stairs. No trailing leads or cables. Staff keep work areas clear, eg no boxes left in walkways, deliveries stored immediately.	Better housekeeping in staff kitchen needed, eg on spills. Arrange for loose carpet tile on second floor to be repaired/replaced.	All staff, supervisor to monitor Manager	From now on xx/xx/xx	xx/xx/xx xx/xx/xx
						Hint, tab here for new row

You should review your risk assessment if you think it might no longer be valid (eg following an accident in the workplace or if there are any significant changes to hazards, such as new work equipment or work activities)

For information specific to your industry please go to <http://www.hse.gov.uk>.

For further information and to view our example risk assessments go to <http://www.hse.gov.uk/risk/casestudies/>

Combined risk assessment and policy template published by the Health and Safety Executive 08/14

Health and safety policy

This is the statement of general policy and arrangements for:			<input type="text"/>	(Name of company)
(Name of Employer/Senior manager)			has overall and final responsibility for health and safety	
(Member of staff)			has day-to-day responsibility for ensuring this policy is put into practice	
Statement of general policy	Responsibility of: Name/Title	Action/Arrangements (What are you going to do?)		
Prevent accidents and cases of work-related ill health by managing the health and safety risks in the workplace				
Provide clear instructions and information, and adequate training, to ensure employees are competent to do their work				
Engage and consult with employees on day-to-day health and safety conditions				
Implement emergency procedures – evacuation in case of fire or other significant incident. You can find help with your fire risk assessment at: https://www.gov.uk/workplace-fire-safety-your-responsibilities				
Maintain safe and healthy working conditions, provide and maintain plant, equipment and machinery, and ensure safe storage/use of substances				

Signed: * (Employer)	Date:
----------------------	-------

You should review your policy if you think it might no longer be valid, eg if circumstances change.
If you have fewer than five employees, you don't have to write down your policy.

Health and safety law poster is displayed at (location)
First-aid box is located:
Accident book is located:



Most Serious Infringements

European legislation that came into force in December 2011, introduced the concept of Most Serious Infringements (MSI). These infringements are specified in the regulations and cover a range of transport law areas. If an MSI is detected, operator licensing authorities of member states must consider an operator's and transport manager's good repute.

This briefing note explains the background to the legislation, what constitutes an MSI, the administrative procedures and the potential consequences of an MSI being detected. It should be read by anyone involved in, or responsible for, the management of transport operations, vehicles or drivers as well as those with responsibility for maintaining an operator's licence.

European regulations on operator licensing

Regulation (EC) No 1071/2009 (known as the 'admission to the occupation regulation') outlines the European rules on operator licensing. These regulations apply to member states and their 'competent authorities' – in other words the licensing authority (Traffic Commissioners and the Office of the Traffic Commissioner in GB) – as well as to operators and transport managers. The European regulations only apply to hire or reward operations, but many of the key features are applied to own account operations in GB.

Article 6 of the regulation specifies the requirements for good repute that must be met by both transport undertakings (operators) and transport managers. Paragraph 2(a) states:

"Where the transport manager or the transport undertaking has in one or more member states been convicted of a serious criminal offence or incurred a penalty for one of the most serious infringements of Community rules as set out in Annex IV, the competent authority of the member state of establishment shall carry out in an appropriate and timely manner a duly completed administrative procedure, which shall include, if appropriate, a check at the premises of the undertaking concerned."

The above paragraph refers to two types of event: serious criminal offence convictions and most serious infringement penalties. Serious criminal offences are listed as being those in relation to operating commercial vehicles, namely: drivers' hours and records; weights and dimensions; driver and transport manager CPC; driver licensing; roadworthiness; operator licensing; carriage of dangerous goods; speed limiters; and animal transport. If an operator in GB is convicted of any of these they must notify the Traffic Commissioner, who would look at good repute, and this has been the case for many years. However, the second type of event of most serious infringement penalties, introduced a change as it is very specific as to the nature of the infringement and also meant that a conviction was not necessary to prompt an investigation, just a penalty.

The paragraph goes on to say:

"The procedure shall determine whether, due to specific circumstances, the loss of good repute would constitute a disproportionate response in the individual case. Any such finding shall be duly reasoned and justified."

If the competent authority finds that the loss of good repute would constitute a disproportionate response, it may decide that good repute is unaffected. In such case, the reasons shall be recorded

in the national register. The number of such decisions shall be indicated in the report referred to in Article 26(1).

If the competent authority does not find that the loss of good repute would constitute a disproportionate response, the conviction or penalty shall lead to the loss of good repute."

There are two key points to note here. Firstly, good repute can only be maintained if its loss is deemed a disproportionate response in an individual case. Secondly, Traffic Commissioners must justify their reasons for not finding that good repute is lost in the national register and the number of these instances must be recorded in a report that is forwarded by every member state to the European Commission every two years. No doubt the Commission will be looking at the consistency of responses (or otherwise) across the EU.

The seven deadly sins

Annex IV of the European Regulation (EC) No 1071/2009 lists a total of seven MSIs, hence the phrase 'the seven deadly sins'.

The MSIs are detailed in the table overleaf, according to the legal area. The last column indicates how the MSI would affect an operator's Operator Compliance Risk Score (OCRS). This is explained in more detail later in this note.

Most Serious Infringements

Legal area	MSI description	OCRS straight to red?
Drivers' hours	Exceeding the maximum 6-day or fortnightly driving time limits by margins of 25 per cent or more. Exceeding, during a daily working period, the maximum daily time limit by a margin of 50 per cent or more without taking a break or without an interrupted rest period of at least 4.5 hours.	Yes
Tachographs and speed limiters	Not having a tachograph and/or speed limiter, or using a fraudulent device able to modify the records of the recording equipment and/or the speed limiter or falsifying record sheets or data downloaded from the tachograph and/or the driver card.	Yes
Roadworthiness	Driving without a valid roadworthiness certificate if such a document is required under Community law and/or driving with a serious deficiency of, inter alia, the braking system, the steering linkages, the wheels/tyres, the suspension or chassis that would create such an immediate risk to road safety that it leads to a decision to immobilise the vehicle.	No
Dangerous goods	Transporting dangerous goods that are prohibited for transport or transporting such as goods in a prohibited or non-approved means of containment or without identifying them on the vehicle as dangerous goods, thus endangering lives or the environment to such an extent that it leads to a decision to immobilise the vehicle.	Yes
Driver and operator licensing	Carrying passengers or goods without holding a valid driving licence or carrying by an undertaking not holding a valid community licence.	No
Digital tachograph cards	Driving with a driver card that has been falsified, or with a card of which the driver is not the holder or which has been obtained on the basis of false declaration and/or forged documents.	Yes
Overloading	Carrying goods exceeding the maximum permissible laden mass by 20 per cent or more for vehicles the permissible laden weight of which exceeds 12 tonnes, and by 25 per cent or more for vehicles with the permissible laden weight of which does not exceed 12 tonnes.	No

Administrative procedure

The administrative procedure involves staff at the Office of the Traffic Commissioner considering the details of the MSI and the history of the operator, in order to prepare a summary of the issue and make a recommendation on action to be taken for the Traffic Commissioner's consideration. The recommendation could range from no action to public inquiry, including the possibility that the Vehicle and Operator Services Agency (VOSA) will carry out an investigation into the activity of the operator in order to establish their overall compliance.

Following a MSI, the loss of good repute by an undertaking or transport manager is considered. If the Traffic Commissioner finds that it would constitute an appropriate response, the MSI will result in the loss of repute and, in the case of an undertaking, the revocation of the operator's licence. Where a transport manager has lost repute, they will be declared unfit to manage

transport activities and their Certificate of Professional Competence (CPC) will no longer be valid in any member state. In both cases rehabilitation measures can be outlined, and once completed to the satisfaction of the Traffic Commissioner, repute can be regained.

The European regulations only apply to convictions and penalties against the undertaking and/or transport manager on standard licences. However, in GB Traffic Commissioners will apply MSI principles to restricted licences as well as standard licences, and also where a penalty is imposed on a person other than the undertaking or transport manager, such as drivers. Although these cases fall outside the European regulations, the Traffic Commissioner will view them with equal seriousness when considering the repute of an undertaking.

Effect on OCRS – straight-to-red

There are four MSIs which, if detected, will move an operator's risk score to straight-

to-red, as detailed in the table above. Where one of the MSIs is detected it will initially be dealt with by issuing a prohibition or fixed penalty. The Operator Compliance Risk Score (OCRS) will also move straight-to-red for six months from the date of the event.

However, it is important to remember that those MSIs which do not lead automatically to straight-to-red, could have the same effect in terms of the number of points attributed. Also, where an operator, rather than a driver, is successfully prosecuted by VOSA, the OCRS will automatically move to red for a calendar year from the date of conviction regardless of the offence.

More information on OCRS can be found in FTA's compliance guide *Understanding and managing your OCRS score*, which members can download for free at www.fta.co.uk or contact the Member Advice Centre on 0870 60 50 000.

Freight Transport Association Limited
(a private limited company)
Hermes House
St John's Road
Tunbridge Wells
Kent
TN4 9UZ

Telephone: 01892 526171
Fax: 01892 534989
Website: www.fta.co.uk

Registered in England Number 391957
06.13/ST_13146

ENFORCEMENT SANCTIONS POLICY – MARCH 2017

GENERAL INSTRUCTIONS

ACTION KEY CODES	
<p>AL: Advisory Letter.</p> <p>IN: Inspection Notice (PG35EC).</p> <p>ORN: Offence Rectification Notice – issued to UK vehicles for minor offences.</p> <p>PG9(D): Prohibition Notice (Delayed) - issued to UK vehicles for roadworthiness offences.</p> <p>PG9(I): Prohibition Notice (Immediate) - issued to UK vehicles for roadworthiness offences.</p> <p>TE160: Prohibition Notice - issued to UK vehicles for offences other than drivers hours/records or roadworthiness</p> <p>TE160(DH): Prohibition Notice - issued to UK vehicles for drivers' hours / records offences.</p> <p>GV170: Prohibition Notice issued to foreign vehicles.</p> <p>DG01: Uniform Monitoring Form for the checking of vehicles carrying hazardous goods.</p> <p>DG02: Prohibition issued to vehicles carrying hazardous goods (Health & Safety at Work Act 1974).</p> <p>GV171: Immediate Prohibition Notice - issued to foreign vehicles for roadworthiness offences.</p> <p>PN: Prohibition notice issued under the Health & Safety at Work Act 1974.</p> <p>FP LEVEL: Fixed Penalty Notice – level is dependant on the seriousness of the defect or offence (E: Endorsable offence or defect).</p> <p>FP #: Drivers of GB registered vehicles will only be issued with the fixed penalty notice if they have culpability for this offence.</p>	<p>FUE: Follow Up Enquiries - Further action will be dependant upon outcome of further enquiries but in any case:</p> <ul style="list-style-type: none"> Where a driver: <ul style="list-style-type: none"> (a) is being prosecuted for a serious offence or for more than 3 fixed penalty offences; or (b) intimates that the operator instructed them to commit offences; or (c) receives a fixed penalty for one or more drivers hours offences which fall into these categories: <ul style="list-style-type: none"> - driving of 70 hours or more in a fixed week; - driving of 114½ hours or more in any two consecutive fixed weeks; - driving for 15 hours or more without taking a qualifying 45 minute break or an interrupted rest period of at least 4 hours 30 minutes. <p>then an analysis of the operator's systems shall be carried out to assess operator culpability. Where there is such evidence and / or further offences are found a report will be submitted to the TC.</p> <ul style="list-style-type: none"> For any fraudulent activity or interference device where the operator's system did not prevent the offence occurring, the matter will be referred to the TC. Where the operator has encouraged or participated in a fraudulent activity then prosecution action will be taken against the operator. In such cases all associated lesser offences will be included in the prosecution case. For numerous* or serious* previous drivers hours offences an analysis of the operator's systems will assess operator culpability. Where there is such evidence and / or further offences are found a report will be submitted to the TC or, for cases involving fraudulent activity, for prosecution. Where an operator is deemed solely culpable for an offence any necessary further evidence will be obtained and the matter reported to the TC or if fraudulent for prosecution. A warning letter can be issued if deemed more suitable. Failure to comply with an ORN may result in the matter being reported to the TC. All other matters to be reported to TC unless minor enough to be dealt with by advisory or warning letter. <p><i>* Numerous is four or more offences which equate to a £100 or a £200 fixed penalty. Serious is one or more offences equating to a £300 fixed penalty.</i></p> <p>P: Prosecution. Court deposit to be taken where offender has no verifiable UK address.</p> <p>X: Where possible the offender will be arrested by Police. Where this is not possible, and only where there is sufficient evidence available, then fixed penalty notices will be issued for alternative offences.</p> <p>MSI / VSI / SI: Any offence deemed to be a Most Serious Offence, regardless of any action taken for any such offences during a vehicle encounter an investigation will be carried out to establish any shortcomings in the operators system and the outcome will be reported to the Traffic Commissioner. This process applies to GB licensed vehicles only. VSI and SI offences will also be collated by DVSA</p> <p>N/A: Not Applicable.</p> <p>NFA: No Further Action.</p> <p>VW: Verbal Warning. Examiners will give advice on how to prevent committing the offence.</p> <p>TC: Traffic Commissioner.</p> <p>DSA: Driving Standards Agency.</p> <p>DVLA: Driver and Vehicle Licensing Agency.</p> <p>VRO: Vehicle Registration Office.</p> <p>HSE: Health and Safety Executive.</p> <p>RMTD: Radioactive Materials Transport Division.</p> <p>CLE 2/TT: Vehicle Excise Offences Report.</p>

